

ANALYSIS

Description

It is important to state that the existing conditions present throughout the roadway segment of the Fashion Row Streetscape project are less than ideal to serve as a basis for an additive beautification response. The roadway presents several challenges, particularly with regards to safety, ADA design standards and compliance, site design standards, drainage, and the ability to create a cohesive, balanced and packaged branded environment.

Our direction was to utilize an engineered design plan, issued by the City, as the basis for the Conceptual Streetscape Plan. Inherently, part of our scope implies an analysis of the conditions being proposed therein that may either render the City in a position where it may be open for liability concerns, or in the least, identify observable constraints/issues that merit further questions and/or investigation prior to installation.

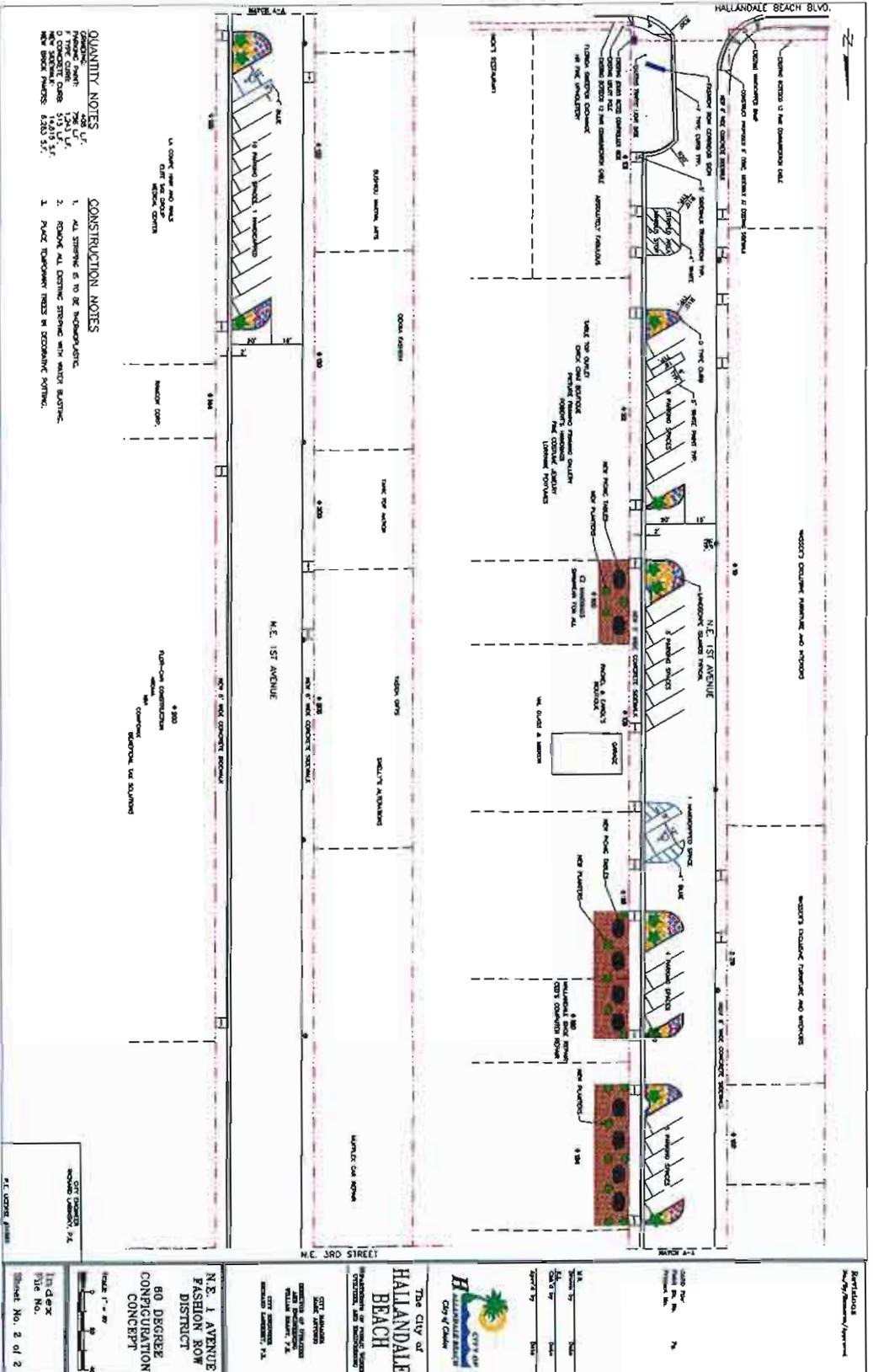
It is our understanding that the improvements proposed in the City-originated engineered design plan are for short-term improvements to be used as a "testing phase", as the direction has been to work around the existing conditions and existing improvements. As such and to be as constructive as possible, we are including some design recommendations that we think may address the issues we raise in the short-term. Nevertheless, it is our recommendation that for the long-term, permanent design of the streetscape, a "whole-package" approach should be considered that addresses all issues of ADA, traffic calming, roadway design and possible center re-alignment, drainage improvements, utility coordination and possible undergrounding, landscape and planting improvements, and all beautification and branding improvements that will create the cohesive 'aesthetic environment' being sought. All these should be done in a manner that support long-term goals affecting parking requirements, zoning, and other land development regulation considerations currently being assessed.

Organization of this Section of the Report

For clarity, each of the enumerated issues being raised is being accompanied with a graphic that identifies an existing example presently found in the project area. The observed constraint/issue will be described in as much detail as possible, and it will be followed with design recommendations we opine are apt as a short-term solution.

Additionally, all recommendations are sequentially enumerated with letters (A, B, C, ...) so they can be easily referenced in the future.

CONCEPTUAL STREETSCAPE PLAN



FASHION ROW
HALLANDALE BEACH, FL
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The City of
HALLANDALE BEACH
DEPARTMENT OF PUBLIC WORKS
OFFICE OF STREETSCAPE DESIGN

STREET NAME
N.E. 1st Avenue
DISTRICT
Fashion Row
CONCEPT
80 Degree Congruence Concept

Scale: 1" = 40'
The City of Hallandale Beach
Sheet No. 2 of 2

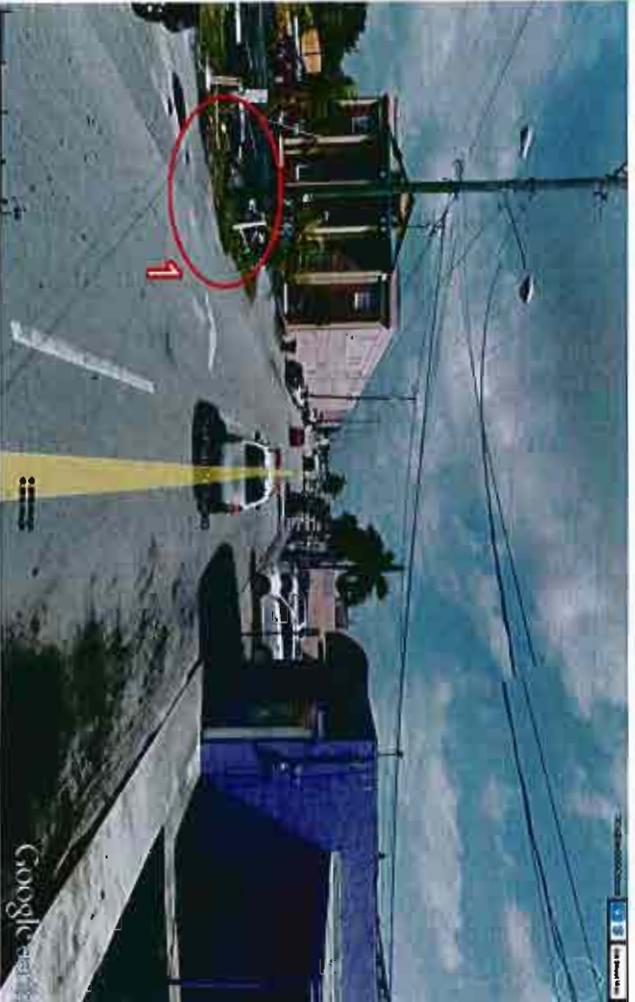
CONCEPTUAL STREETSCAPE PLAN

Item 1:

The City-originated engineered design improvements around existing utility poles to remain. The utility poles are not presently located on the outside edge of the public's ROW, but are offset into the ROW at varies offsets. These provide clearances between the outside boundary of the ROW and the face of the utility pole that vary from 2.96 feet to 4.9 feet, as determined from the provided Cad base file. At times abutting improvements in the private properties may restrict the minimum clearance of 3.0 feet required by ADA standards.

Recommendations:

- A. Conduct an in-field assessment to ensure that all areas where existing utility poles exist adequately provide the minimum clearances required to meet ADA standards.
- B. Make notations on the construction plans to ensure that the contractor provides a minimum of a 3-foot clearance to meet ADA requirements.



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Item 2:

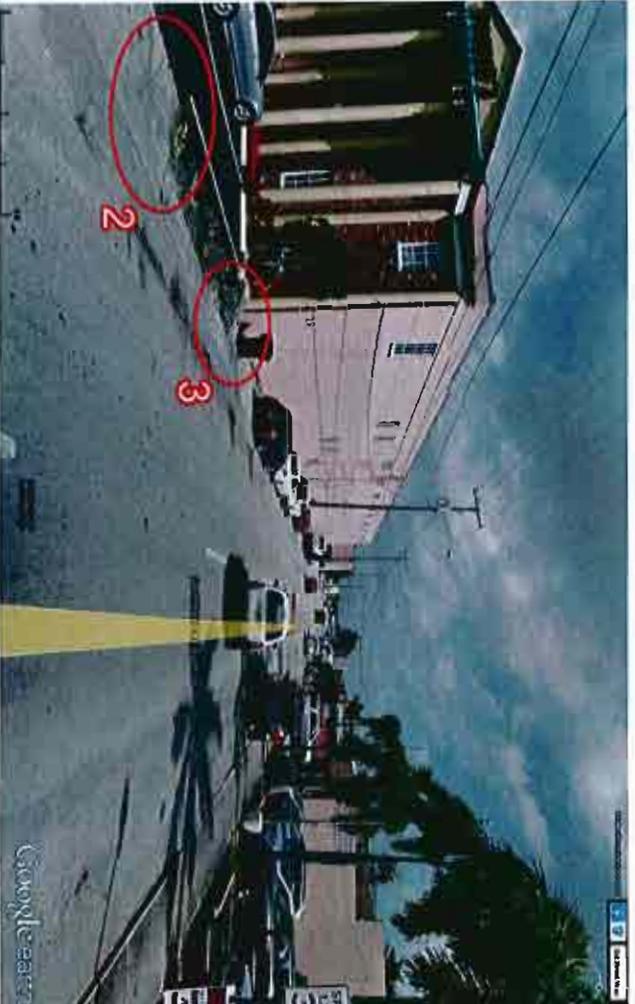
The City-originated engineered plan proposes improvements that are in conflict with the existing elevations of present on-site. The engineered plans propose a 6 foot wide sidewalk be installed abutting the edge of the ROW, which coincides with the edge of the building's street-facing facade. The proposed sidewalk on the plans does not indicate any running-slope pitch to meet the existing grade of the building's parking lot, which is estimated at approximately 12-14 inches above the finish grade of the abutting asphalted edge.

Item 3:

Similar to the issue in Item 2, there is a drop-off between the finish grade of the parking lot at the building's edge, which is approximately 14-18 inches above the finish grade of the asphalted area.

Recommendations:

- C. Provide a topographical to better determine the elevation constraints and drainage patterns on-site
- D. Raise the sidewalk elevation as it approaches the parking lot entrance so as to ensure that the cross slope of the proposed sidewalk does not exceed the 2 percent cross-slope maximum allowed by ADA standards.
- E. Because the distance to obtain the necessary elevation rise in the sidewalk is very short immediately south to the area encircled as "2", it may be necessary to utilize an ADA ramp design with a maximum slope 8.33% with handrails, otherwise we recommend providing access ramps at a maximum of 4.5% (allowing for construction error) to minimize on the need of having to provide handrails and any limitations on lengths of slope runs.
- F. Where abutting elevation changes exceed 18" within a 3-foot buffer of the ramp/slope, handrails should be provided so pedestrians do not fall into the adjacent proposed travel lane.



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Item 4:

The same condition as that identified in Item 3 exists at this northern corner of the building. Additionally, there is the presence of a utility pole that may pose additional constraints to the construction of an access ramp/grade that satisfies ADA requirements.

Item 5:

The same condition as that identified in Item 2 exists at this access to the loading yard of the property.



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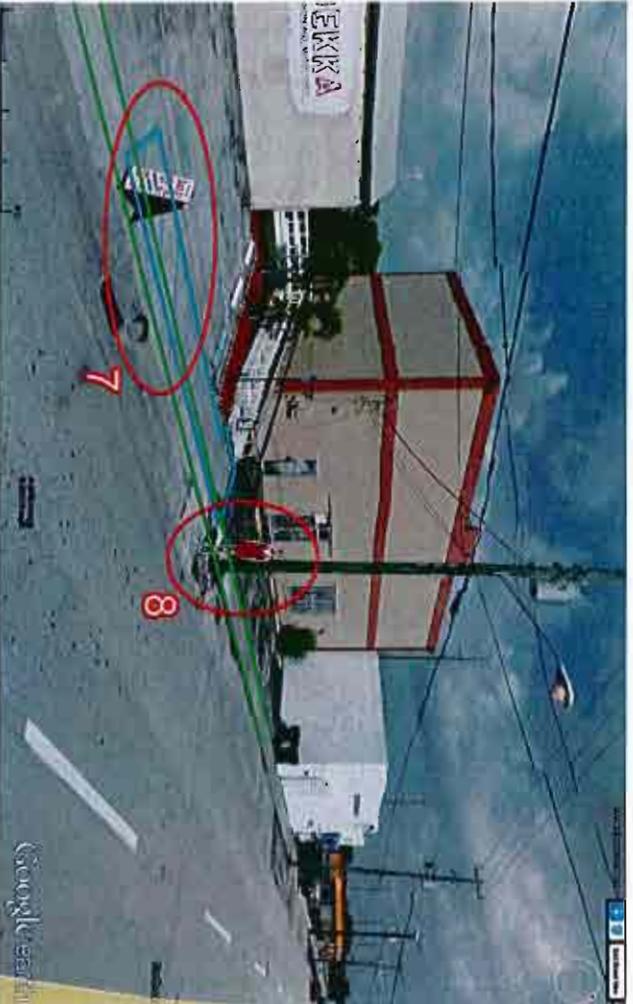
Item 6:

The City-originated engineered plan proposes the installation of a 6-foot wide sidewalk along the outermost edge of the ROW (shown in green). Presently, this area is being utilized by several properties as the location for their 90-degree, pull-in parking. In several of these situations, the designs of the parking stalls meet the dimensional design standards at a minimum and at times are sub-standard. By providing a sidewalk in this location, already-challenged parking areas will be rendered sub-standard, and the proposed sidewalk will invariably be obstructed by parked vehicles.

Recommendations:

- G. As a first step, the land development regulations should assess the need that requires each property to provide a minimum number of parking spaces, and what those minimum parking spaces requirements should be. Otherwise conflicts in the proposed improvements, both in the short-term and long-term realms will be most challenged by parking requirements. There may be a need to lessen the number of parking requirements, at a minimum meeting the needs of ADA Standards.
- H. The City should conduct a short-term and long-term parking study to determine the feasibility of shared parking standards throughout the roadway segment coupled with parking count improvements provided by the City throughout the ROW or elsewhere in the immediate, walkable vicinity.





Item 7:

Similar to the parking conflicts in Item 6, the City-originated engineered plan proposes the continuation of a sidewalk alongside the edge of the ROW. At times, the conflicts with the existing improvements may not only be of a spatial, site design one, but they also provide aesthetic implications. As an example, the area outlined in blue identifies the existing ADA access route from the ADA parking space to the access ramp. By code, this route has to be striped and marked with blue indicators lines in accordance to design standards established by the ADA code. The area shown in green illustrates the location of the proposed sidewalk, which is envisioned to be paved with a branding paving design. In order to meet code, the blue striping will have to be installed on top of the paving design, which will create an aesthetic conflict to the 'look' and 'finish quality' of the environment. This situation will be typical at all locations where the proposed paved sidewalk will conflict with any parking's striping.

Item 8:

The City-originated engineered plan proposes a 16-foot wide travel lane immediately abutting the edge of the sidewalk. We have concerns about this design and potential safety considerations for both pedestrians and drivers – considerations that may put the City at a liability risk. In essence the problem is two-fold:

- Pedestrians will be walking on a sidewalk that is flush with and immediately adjacent to the roadway. There are not mechanisms in the design plan to ensure that if a vehicle veers from their path that the pedestrian will be protected.
- Immediately along the edge of the travel lane, vehicle will be confronted with existing utility poles with no mechanism to prevent a vehicle from crashing into them

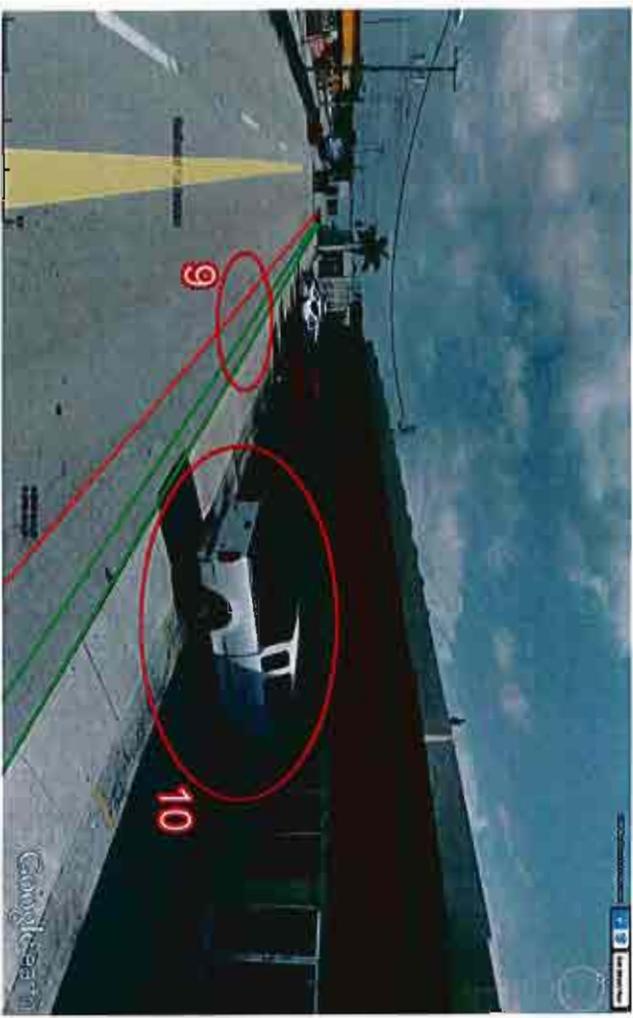
Recommendations:

1. The posted speed limit is currently at 30 mph. The city should make every attempt to further reduce the speed limit.
- J. The travel proposed lane should be reduced from 16-feet to 12- feet wide, or even 11-feet if possible.
- K. Once the travel lane has been reduced, the remaining 4 or 5 feet of asphalt (depending on the width of the travel lane) between the travel lane and the sidewalk and poles should be treated as a 'warning strip'. This warning strip should be colored and textured so if a vehicle veers off of the travel lane there is every attempt made to alert the driver to prevent any impact to pedestrians or utility poles.
- L. As an added caution, a collar curb should be provided around the bases of all existing utility poles to protect vehicular drivers from accidently having a collision with the poles.
- M. Where possible and where parking/driveway access is not needed, the City should provide bollards alongside the edge of the sidewalk in the 'warning strip' area to provide as much protection for pedestrians on the sidewalk.



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Item 9:

The City-originated engineered plan proposes the installation of a 2-foot wide gutter in the areas shown in green. Additionally, the shown orange line identifies the location of a swale area that has been paved over (the orange line indicating the low-point of the swale). There is a redundancy in the provision of two drainage systems (swale and gutter) that will only amount to unnecessary short-term expenses.

Item 10:

Similar to the issue identified in Item 6, the conflicts of sub-standard parking and the obstructions that parked vehicles pose remain unaddressed. Furthermore, the safety concern of a pedestrian, in an attempt to walk around an obstructing vehicle, must enter into the roadway without any protection, in our opinion, puts the City at a severe risk of being liable if a vehicle collides with a pedestrian.

Recommendations:

- N. Determine the drainage needs of the roadway to assess if a gutter is truly necessary.
- O. Reduce the number of lanes at the northernmost end of the corridor to two (2) lanes, instead of three, and provide the necessary striping to clearly mark the changes.
- P. Reduce the width of the travel lanes to 12 feet. This will alleviate some breathing room between the travel lanes and the sidewalks.
- Q. Provide a 'warning strip', consistent with Recommendation K, on both sides of the road, to provide greater safety for the pedestrians and vehicles.

CONCEPTUAL STREETSCAPE PLAN



OVERALL STREETSCAPE PLAN VIEW



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DETAIL STREETSCAPE PLAN VIEW

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DETAIL STREETSCAPE PLAN VIEW

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DETAIL, STREETSCAPE PLAN VIEW

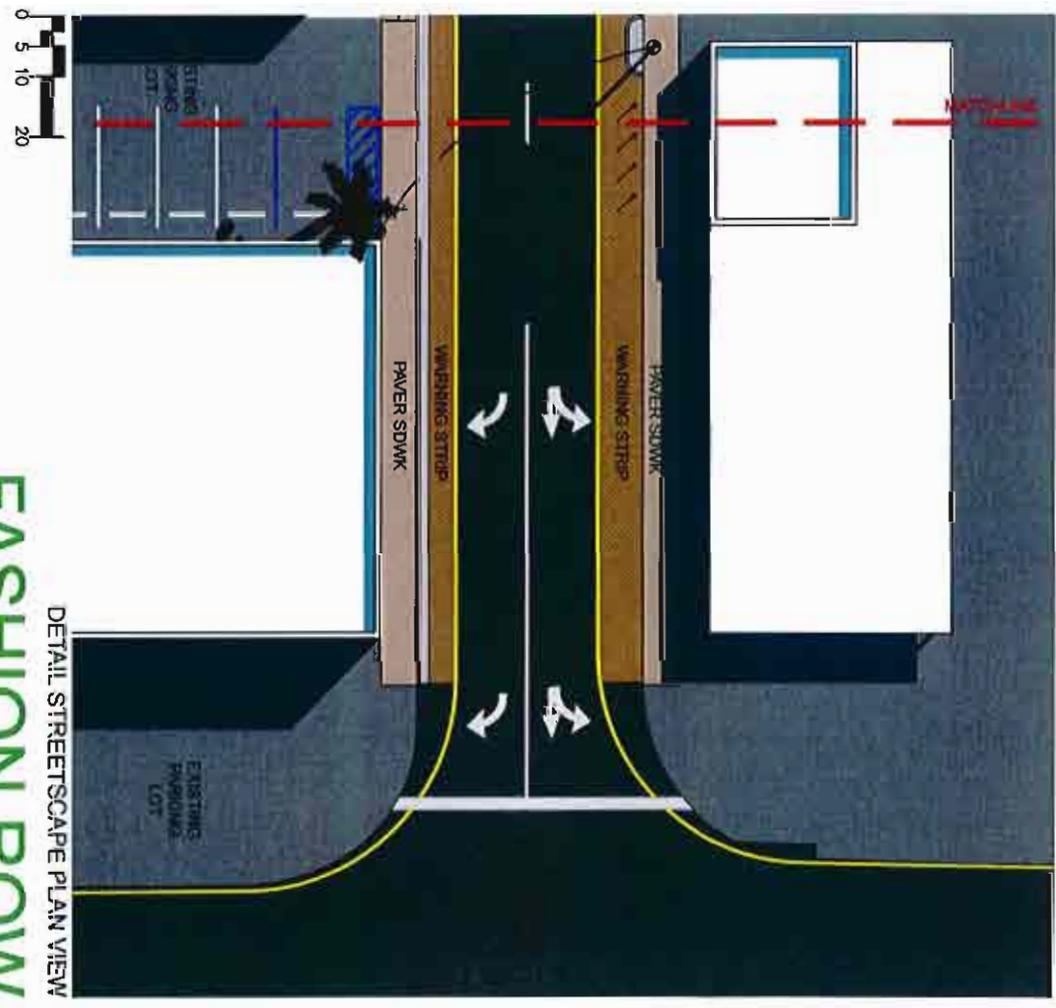
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DETAIL STREETSCAPE PLAN VIEW

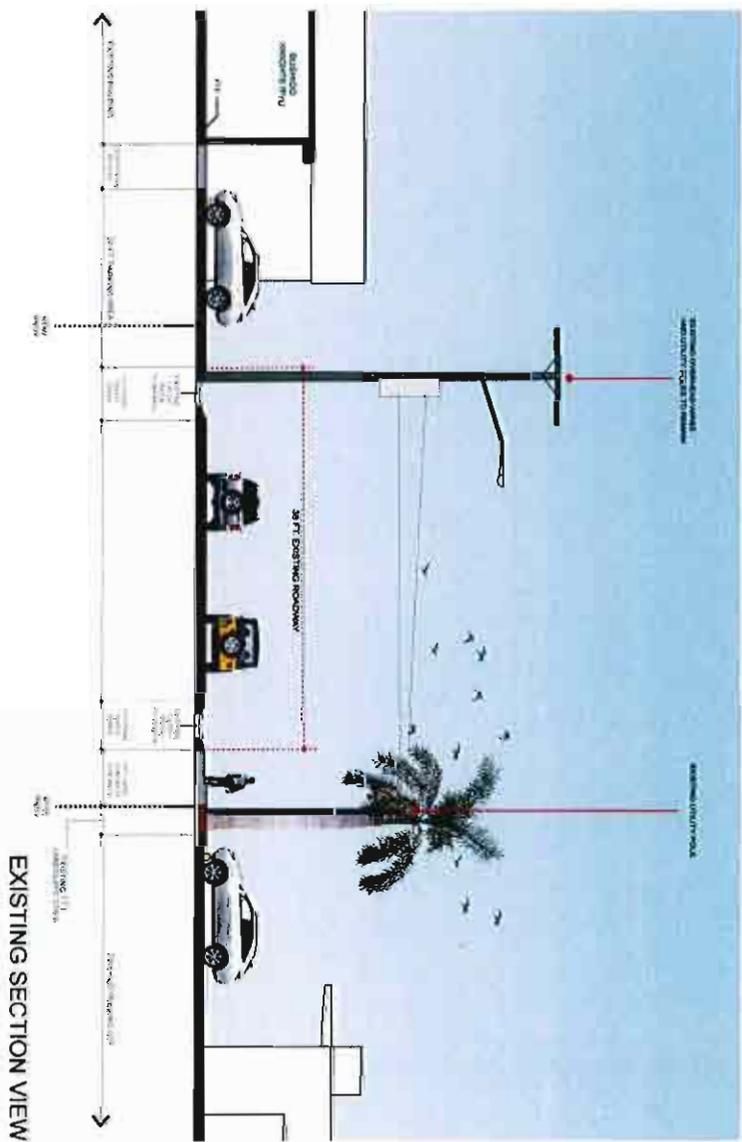
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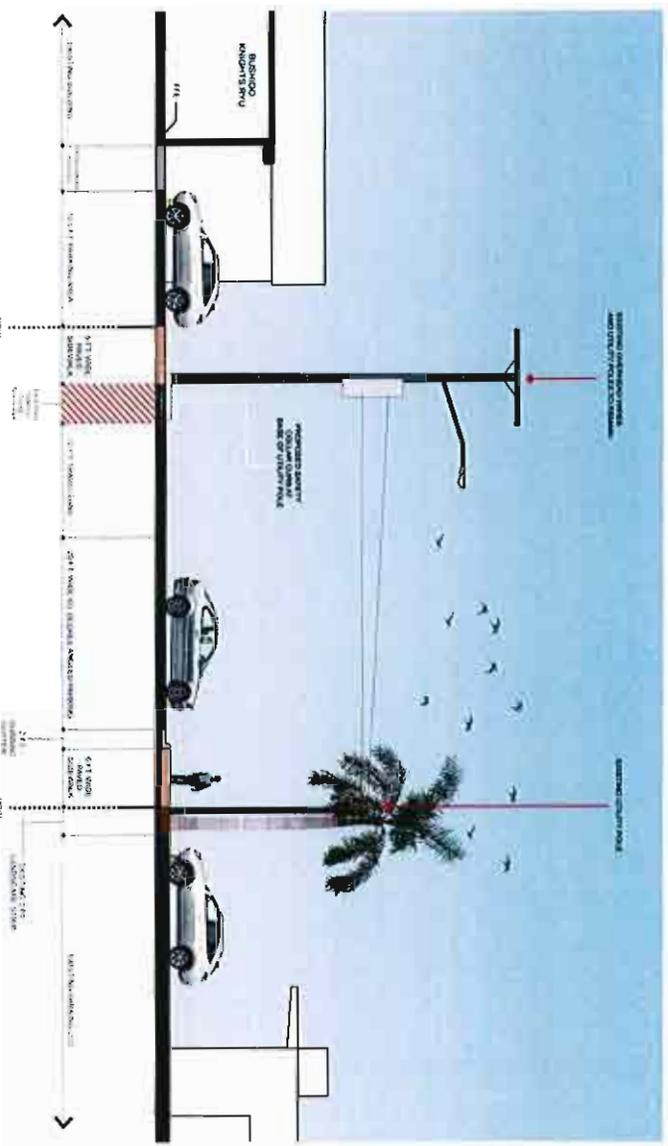
DETAIL STREETSCAPE PLAN VIEW
FASHION ROW
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 HALLANDALE BEACH, FL
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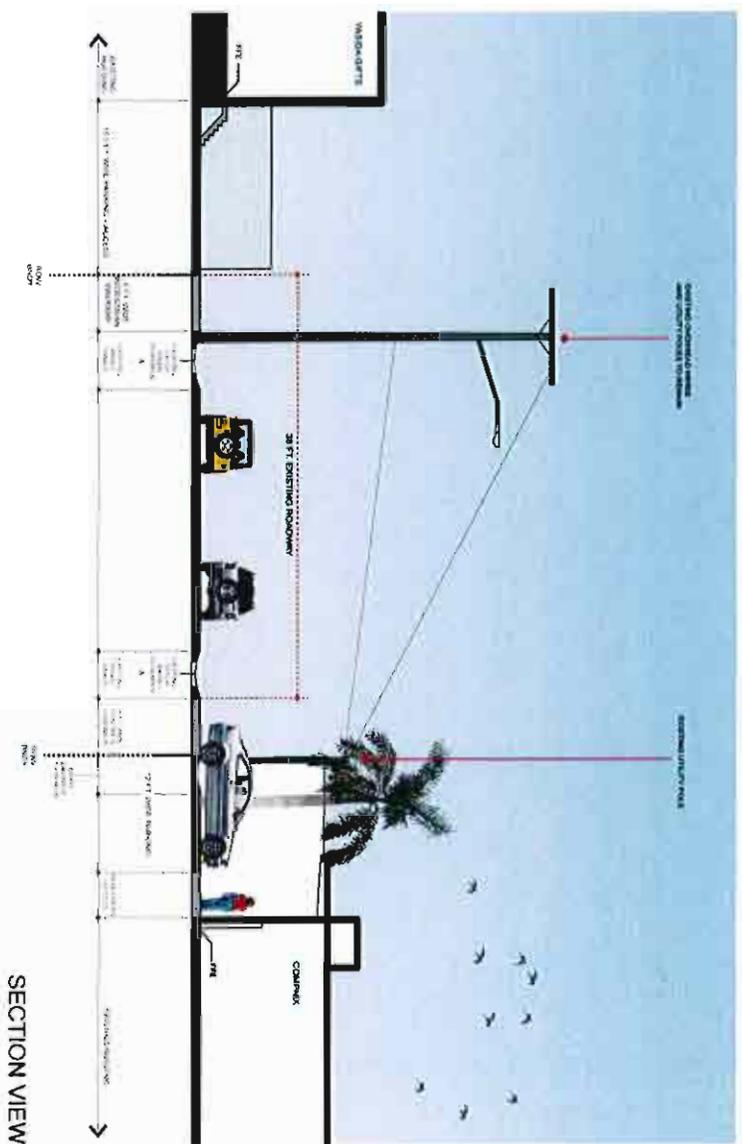
PROPOSED SECTION VIEW



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SECTION VIEW



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MANUFACTURER LANDSCAPE FORMS
MODEL 35: STAY (BACKLESS)
COLOR BLACK
INSTALLATION EMBEDDED
ACCESSORIES CENTER-ARM



MANUFACTURER LANDSCAPE FORMS
MODEL CHASE PARK
COLOR BLACK
INSTALLATION SURFACE MOUNT



MANUFACTURER LANDSCAPE FORMS
MODEL SENTINEL
COLOR BLACK
INSTALLATION EMBEDDED

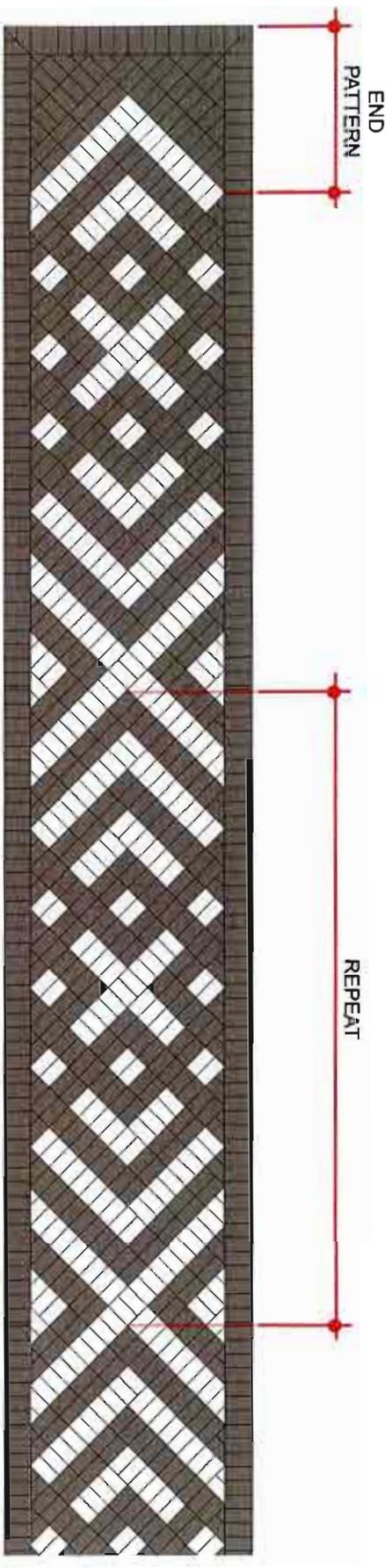


MANUFACTURER BEACON PRODUCTS
LUMINAIRE 26" URBAN LUMINAIRE (LED)
MODEL MIRAMAR DS
ARM STRUT ARM
BASE MERIDIAN M
POWER SUPPLY SEPA85 SOLAR POWER ASSEMBLY
ACCESSORIES BANNER ARMS
COLOR BLACK



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6' WIDE SIDEWALK - PAVING PATTERN DESIGN

MANUFACTURER HANSON PAVERS
PATTERN 45-DEGREE HERRINGBONE
DIMENSION 4" X 8" STANDARD
INSTALLATION SAND-SET
COLOR - LIGHT P2
COLOR - DARK E8

CONCEPTUAL STREETSCAPE PLAN

GATEWAY PLANTING PALETTE



PHOENIX SYLVESTRIS [8' CLEARANCE]



SCHEFFLERA ARBORICOLA



CRINUM AUGUSTUM



NEOREGELIA 'FIREBALL'

CORRIDOR PLANTING PALETTE



BULNESIA ARBOREA [4.5' CLEARANCE] (WHERE NO UTILITY CONFLICTS EXISTS)



LIGUSTRUM JAPONICUM (WHERE CONFLICTS EXIST WITH OVERHEAD UTILITIES)



RHAPHIOLEPIS INDICA



DANELLA TASMANICA

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